

Lied Pedestrian Bridge Superstructure Repair

Papio-Missouri River NRD Project 20200403

Invitation for Bids

The District is seeking bids for the referenced project. The project consists of concrete repair, repair of rail sections and connectors, fabrication of new connectors, and reinstallation of rail sections and connectors. Lied Pedestrian Bridge Superstructure Repair project plans and information are enclosed.

If your company has an interest in bidding on the proposed work, please complete the enclosed bid form and deliver or mail to the Papio-Missouri River NRD, 8901 S. 154th St., Omaha, NE 68138 by 10:00 a.m., local time on August 18, 2020.

A project pre-bid site showing will be held on August 13, 2020 at 10:00 a.m. at the south/west end of the bridge in Cass County, accessible from Hwy 66 near South Bend, NE.

If you have any questions, please contact

Eric Williams
Natural Resources Planner
Papio-Missouri River NRD
EWilliams@PapioNRD.org
(402) 315-1704

Lied Pedestrian Bridge Superstructure Repair

Papio-Missouri River NRD Project 20200403

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Lied Pedestrian Bridge Superstructure Repair

Papio-Missouri River NRD Project 20200403

Project Information – August 3, 2020

SECTION 1

MOBILIZATION / DEMOBILIZATION

PART 1 - GENERAL

1.1 SUMMARY

- A. Section Includes:
 - 1. Mobilization of Contractor's forces and construction equipment onto site.

1.2 DESCRIPTION

- A. Mobilization/demobilization shall include mobilization/demobilization of all construction equipment, materials, supplies, appurtenances, and the like, manned and ready for commencing and completing the Contract Work.
- B. Mobilization shall also include; assembly and delivery to the Work Site equipment, materials, and supplies necessary for the execution of the work; the clearing of and preparation of the Contractor's Area; the complete assembly, in working order, of equipment necessary to perform the required work; and for all personnel services preparatory to commencing the actual work.
- C. Demobilization shall include the removal from the Work Site upon completion of the Project of all construction equipment, materials, supplies, appurtenances and the like, that were brought on the site and not incorporated into the Project.

PART 2 - PRODUCTS - (NOT APPLICABLE TO THIS SECTION)

PART 3 - EXECUTION - (NOT APPLICABLE TO THIS SECTION)

END OF SECTION

Lied Pedestrian Bridge Superstructure Repair

Papio-Missouri River NRD Project 20200403

Project Information – August 3, 2020

SECTION 2 RAILING AND DECK REPAIRS

PART 1 - GENERAL

1.1 SUMMARY

A. Repair to concrete damage, repair necessary for existing rail segments to be reinstalled using the existing rail connectors which are installed into the bridge deck, fabrication of new connectors to replace equipment which was damaged, and installation of repaired rail sections.

1.2 DESCRIPTION

- A. Repair concrete damage and remove excess concrete from the site.
- B. Repair rail and fence sections which are stored at the south/west end of the site.
- C. Repair fence mesh and timber panels and reattach to the railing support structure in order for the combined fence unit to function as originally designed and installed.
- D. Connectors which were damaged and unable to be reused shall be replaced.
- E. Reassembled rail sections shall be reinstalled using existing and replaced connectors.

PART 2 - PRODUCTS

2.1 Refer to plan sheets for detailed information about products to be used.

PART 3 - EXECUTION

3.1 Refer to plan sheets for information about practices to be observed.

END OF SECTION

Lied Pedestrian Bridge Superstructure Repair

Papio-Missouri River NRD Project 20200403

Project Information – August 3, 2020

SECTION 3 SPECIAL PROVISIONS

1.0 GENERAL PROJECT INFORMATION

1.1 Description

The LIED Pedestrian Bridge incurred damage from debris impacts during a high water flood event. This project is to repair the bridge railing and selected concrete spalls as noted in the plans.

1.2 Access

Access to the bridge and for all repair work shall be from the west side (Cass County). No access from the east side (Sarpy County) is available due to local damage to trail and ongoing restoration work.

Equipment on the bridge shall have rubber tires or rubber tracks. No metal track equipment is allowed in direct contact with the concrete deck. Contractor may elect to lay down wood sheeting to protect the concrete deck if metal track equipment is to be used.

Vehicles or equipment on the bridge shall be minimized. Any equipment used in the repair work on this project shall not have a gross weight in excess of 50,000 lbs.

The bridge is currently closed to all pedestrians and vehicular traffic and shall remain closed to all traffic outside of contractor equipment and personnel. The locked entrance wall on both sides of the trail (east and west) shall be secured at the end of each night to ensure no one access the trail bridge.

1.3 Handling and Disposal of Materials

Lied Pedestrian Bridge Superstructure Repair

Papio-Missouri River NRD Project 20200403

Project Information – August 3, 2020

SECTION 4 CONTRACT CONSIDERATIONS

PART 1 - GENERAL

- 1.1 CONTRACTOR to complete Bid Form and return to OWNER,

Papio-Missouri River NRD
8901 S. 154th St.
Omaha, NE 68138
- 1.2 Bid forms will be received by 10:00 a.m. local time on August 18, 2020.
- 1.3 The selected CONTRACTOR will be required to sign the attached Contractor Agreement.
- 1.4 The WORK will be substantially complete by September 25, 2020.
- 1.5 The CONTRACTOR is required to adhere to project specifications in Section 1 through Section 3 of Project Information.

END OF SECTION

All material resulting from the removal of specified bridge components shall become the property of the Contractor and shall be promptly removed from the right-of-way. If debris should enter the channel below, it shall be removed at the end of each construction day.

2.0 REPAIR RAIL SECTIONS

Sixty (60) 8'-0" rail sections, as detailed in the plans, were damaged by the flood debris and removed from the bridge and stockpiled locally. 19 of the 60 sections removed sustained damage to timber and steel elements of the rail section. The degree of damage on each rail section varies from one element to numerous elements. The Contractor is encouraged to attend the site visit to get a comprehensive feel for the level of expected repairs.

Any replacement of metal or timber elements shall follow closely the existing dimensions and material characteristics of the element. The plans provide the material properties of expected elements to be replaced. Existing plans of the bridge railing are available should other elements be determined damaged and in need of replacement. Any newly identified damage outside the scope of these plans shall be brought to the attention of the Engineer and Papio-Missouri River NRD for determination of action.

3.0 REPAIR RAIL CONNECTORS

50 galvanized metal connectors attaching the rail section to the edge of the concrete deck have been damaged and in need of repair. The galvanized connectors have minor bends in the ¼" metal plates that may be repaired by cold bending.

Connectors with missing bolts or threaded rod inserts into the concrete deck shall be replaced with like sized high strength (HS) bolts or threaded rod. All HS bolts or threaded rod must comply with ASTM A153 galvanizing procedures.

4.0 FABRICATE NEW CONNECTORS

New metal elements for the rail panels or rail connectors shall be from structural steel in accordance with ASTM A709, Grade 50.

All completed steel connection components shall be galvanized in accordance with ASTM A123.

Details of existing connectors can be found in the AS-BUILT plans for the LIED Bridge and available upon request.

5.0 REINSTALL RAIL SECTIONS

The reinstallation of the rail sections and rail connectors onto the LIED Bridge shall be covered under the pay item “REINSTALL RAIL SECTIONS”.

The installation of the rail sections shall be performed with care to avoid additional damage to the concrete deck. The rail shall be erected in straight and vertical alignment with the existing railing and concrete deck. The rail shall be plumbed in both vertical and horizontal directions to the satisfaction of the field inspector.

The connection of the rail connectors to the edge of the concrete deck may be performed by one of the following methods:

- A. Grout Existing Connector
 - i. Many of the rail connectors are still attached to the rail section posts and have the bolt or rod for insertion into the concrete deck still functional. These connectors shall be allowed to be reattached with existing materials. The insertion hole in the concrete deck shall be cleaned of all debris and concrete dust prior to injecting epoxy grout and insertion of the connectors.
- B. Install New Connector
 - i. Connectors that are missing a connection bolt or threaded rod for connection to the concrete deck shall have a new bolt or threaded rod installed. Contractor may elect to weld new bolted connectors to the plate or may drill and insert threaded rod through the connector plates.

6.0 CONCRETE REPAIR

All concrete areas spalled and requiring repair are listed in the plan set. Repaired areas will be measured for payment to the nearest SF.

These concrete repairs shall be repaired with an epoxy grout capable of attaining a strength of 3,000 PSI. The epoxy grout shall be one selected from the Approved Products List found on the Nebraska Department of Transportation web site. Manufacturer’s product data sheet of the selected product shall be shown to the field inspector for verification.

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BID FORM

Items of work for the project are as follows:

Item No.	Bid Item Description	Estimated Quantity	Units	Unit Price	Total Amount
1	Mobilization	1	LS		
2	Concrete Repair	12	SF		
3	Repair Rail Sections	19	EA		
4	Repair Rail Connectors	50	EA		
5	Fabricate New Connectors	4	EA		
6	Reinstall Rail Sections	60	EA		
	Total Bid (Items 1 thru 6)	Tax Exempt *			\$

*Do not include Nebraska State or Local Sales Tax in your bid. The District is Tax Exempt.

LS = Lump Sum SF = Square Feed EA = Each

DATE SUBMITTED:

COMPANY NAME:

COMPANY ADDRESS:

COMPANY REPRESENTATIVE:

Print Name

Signature

COMPANY REPRESENTATIVE TELEPHONE NO:

COMPANY REPRESENTATIVE EMAIL:

Contractor Agreement (this “Contract”)

Papio-Missouri River Natural Resources District (“the District”)
8901 S. 154th Street
Omaha, NE 68138
Phone: 402-444-6222
Fax: 402-895-6543

Contractor Information:

Name: _____ (“Contractor”)
Address: _____
City/State/Zip: _____
Phone: _____ Fax: _____

Work to be performed for the District (the “Work”):

Contract Price: _____

Contractor shall commence the Work on _____, and shall complete the Work by _____. Time is of the essence for the Work. Contractor shall be responsible for obtaining any and all licenses, permits, and authorizations required for the Work. This Contract may only be amended or supplemented by a written change order executed by the District. Contractor shall not be entitled to any additional compensation in excess of the Contract Price, without The District’s prior written approval in the form of a written change order.

Contractor agrees to undertake the Work as an independent contractor without creation of an employee-employer relationship between Contractor and the District. To the fullest extent permitted by law, Contractor will defend, indemnify and hold harmless the District, its directors, officers and employees, from and against all claims and demands of all persons arising out of the performance of the Work including but not limited to claims by Contractor, Contractor’s employees, and/or third parties for damages to persons or property, except as may be caused directly by the sole negligence or willful misconduct of the District or of its directors, officers or employees. Contractor shall not subcontract any portion of the Work or assign any portion of this Contract without the prior written consent of the District, which consent may be withheld in the District’s sole discretion.

Contractor agrees to and shall maintain the following types and minimum limits of insurance during the term of this contract, and at the commencement of the Work and at other times as reasonably requested by the District, to furnish Certificates of Insurance to the District evidencing the same:

- A. Commercial General Liability - \$1,000,000 – naming the District as an additional insured
- B. Workers Compensation – Statutory limits
- C. Commercial Auto Liability - \$1,000,000

Contractor affirmatively waives any and all claims by Contractor against the District for indirect or consequential damages, including but not limited to, lost or anticipated profits. Contractor further hereby waives any and all lien rights it may have with respect to the Work and/or any property related to the Work.

Contractor agrees to perform the Work skillfully, carefully, diligently and in a good and workmanlike manner, and guarantees and warrants the Work against all defects in materials or workmanship for 2 years after completion of the Work. Contractor agrees to comply with all Federal, State and local laws, codes, regulations, and the policies of the District, which can be found at <http://www.papionrd.org/about-nrd/policies-and-manuals/> (collectively, "Laws and Regulations"). Contractor further agrees to pay all taxes imposed by any Federal, State or local law, and any employment insurance, pensions or old age retirement funds, due as a result or incident of the Work.

Contractor shall submit a written invoice to the District for the Work completed up to the date of the invoice. The District will review the invoice to determine whether the invoice accurately represents the amount completed and, in its sole discretion, determine whether to approve the full invoice or pay a portion of the invoice for the Work completed at the date of the invoice. The invoiced amount as approved by the District will be paid to the Contractor no later than forty-five (45) days after the date the invoice was received by the District.

The District may terminate this Contract, for its convenience, at any time and for any or no reason upon seven (7) days prior written notice to the Contractor. In the event of Contractor's material breach of the Contract, the District reserves the right to immediately terminate the Contract without prior notice to Contractor. If the Contract is terminated, Contractor shall only be entitled to the portion of the Contract Price for which Contractor has performed the Work. The District reserves any and all rights in the event of termination of the Contract.

Pursuant to Neb. Rev. Stat. § 48-1122, Contractor and his subcontractors shall not discriminate against any employee or applicant for employment, to be employed in the performance of this contract, with respect to his or her hire, tenure, terms, conditions, or privilege of employment because his or her race, color, religion, sex, disability, or national origin.

Pursuant to Neb. Rev. Stat. § 4-114, Contractor shall utilize a federal immigration verification system to determine the work eligibility status of new employees physically performing services related to this contract.

Nothing herein or in any other agreement between Contractor and the District shall be construed as a waiver of all or any part of, or as in any way limiting, the sovereign immunity afforded to the District pursuant to Laws and Regulations.

Accepted and agreed to:

Papio-Missouri River NRD

Contractor: _____
(Business Name)

By: _____
(Signature/Date)

By: _____
(Signature/Date)

(Print Name/Title)

BRIDGE DIVISION.

COMPUTER****

DATE*****

DGN SPEC*****

PAPIO-MISSOURI RIVER NATURAL RESOURCES DISTRICT

PLANS FOR CONSTRUCTION

LIED PEDESTRIAN BRIDGE SUPERSTRUCTURE REPAIR

SHEET INDEX

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	GENERAL OVERVIEW
3	RAILING DETAILS
4	CONCRETE REPAIR DETAILS

BID ITEMS

MOBILIZATION	_____	1	LS
CONCRETE REPAIR	_____	12	SF
REPAIR RAIL SECTIONS	_____	19	EA
REPAIR RAIL CONNECTORS	_____	50	EA
FABRICATE NEW CONNECTORS	_____	4	EA
REINSTALL RAIL SECTIONS	_____	60	EA



ENGINEER CONTACT INFORMATION:

THE SCHEMMER ASSOCIATES, INC.
1044 N 115TH, SUITE 300
OMAHA, NE 68154
(402) 493-4800

REVIEWED FOR CONSTRUCTION:

PAPIO-MISSOURI RIVER NRD
8901 S. 154TH ST
OMAHA, NE 68138
PHONE (402) 895-6543

MARLIN PETERMANN P.E.
ASSISTANT GENERAL MANAGER

SHEET NO. 1

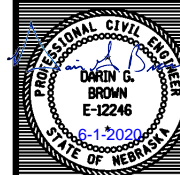
PROJECT NUMBER

LIED BRIDGE



PAPIO-MISSOURI RIVER
NATURAL RESOURCES
DISTRICT (PAPIO NRD)

DESIGNED BY DGB DETAILED BY CJF CHECKED BY JTP DATE MAY 2020
TITLE AND QUANTITY SHEET
LIED BRIDGE SUPERSTRUCTURE REPAIR



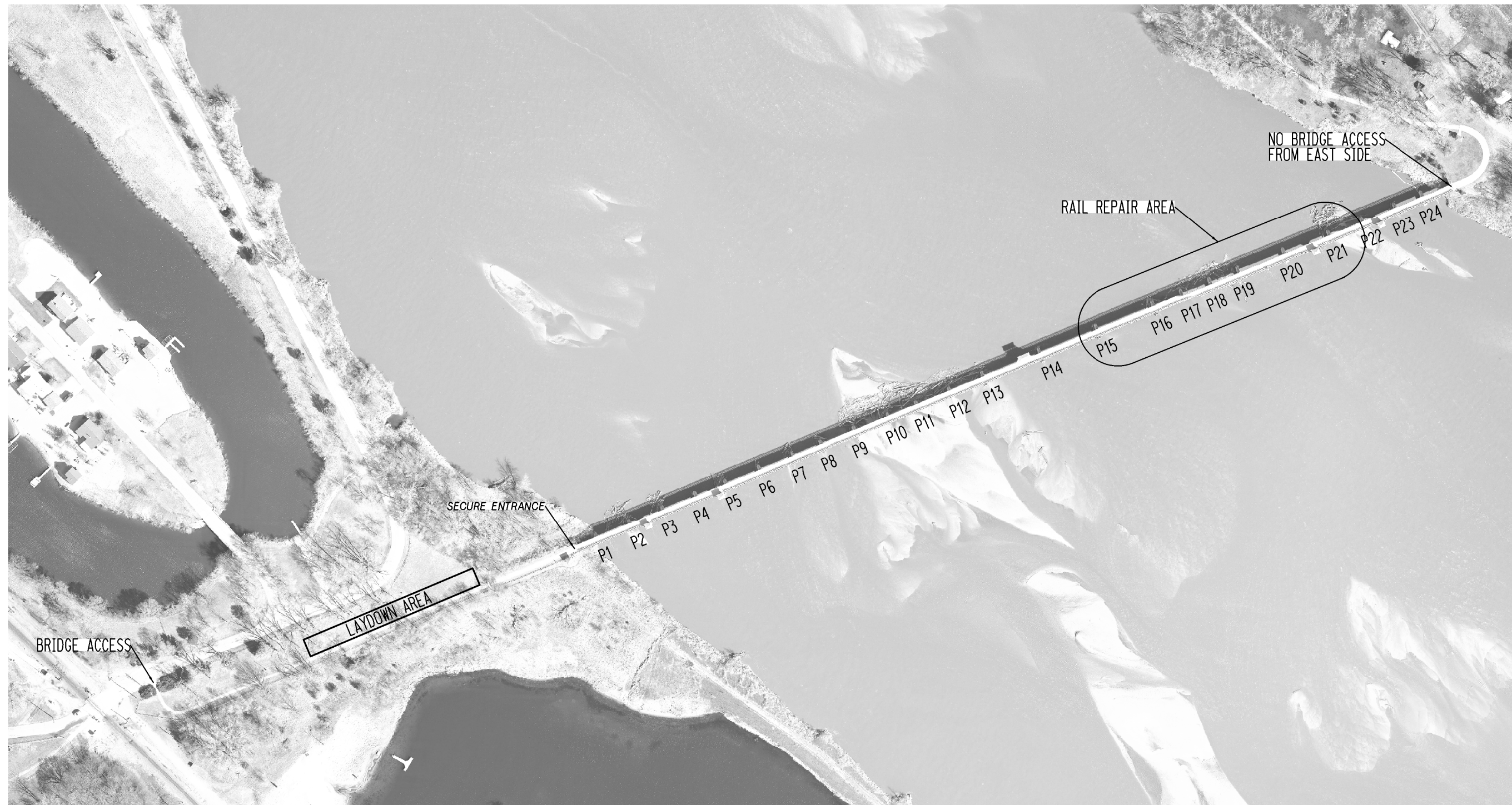
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BRIDGE DIVISION

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DATE*****

DGN SPEC*****



GENERAL NOTES

Access to the bridge shall be from the west side (Cass County).
No access from the east side (Sarpy County).

All repairs are to be made from the concrete deck. No work platforms or equipment will be allowed in the water. Any materials that fall into the water shall be removed at the end of each day.

The designated laydown area may be used for equipment storage and/or repair work area. Any damage to rock trail or grass shall be restored to preexisting conditions.

The west entrance to the bridge shall be secured each night to block all access to pedestrians. The existing wood gates and locks shall be maintained.

Rail repairs are intended for the upstream side of the bridge only. No corrective work is necessary to any rail on the downstream side.

SHEET NO. 2

PROJECT NUMBER

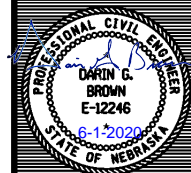
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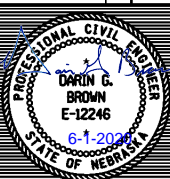
PAPIO-MISSOURI RIVER
NATURAL RESOURCES
DISTRICT (PAPIO NRD)

GENERAL OVERVIEW

DESIGNED BY DGB DETAILED BY CJF CHECKED BY JTP DATE MAY 2020
LIED BRIDGE SUPERSTRUCTURE REPAIR



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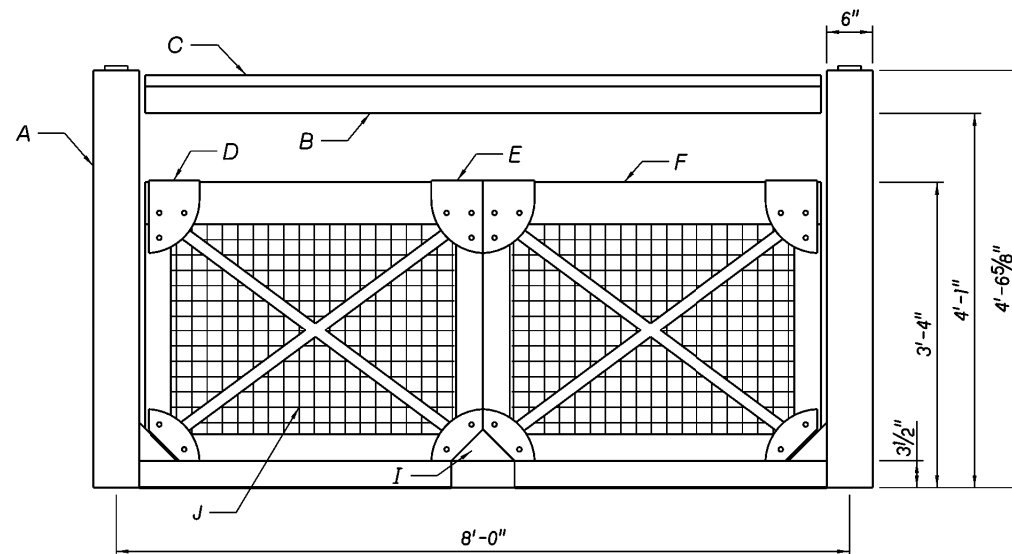
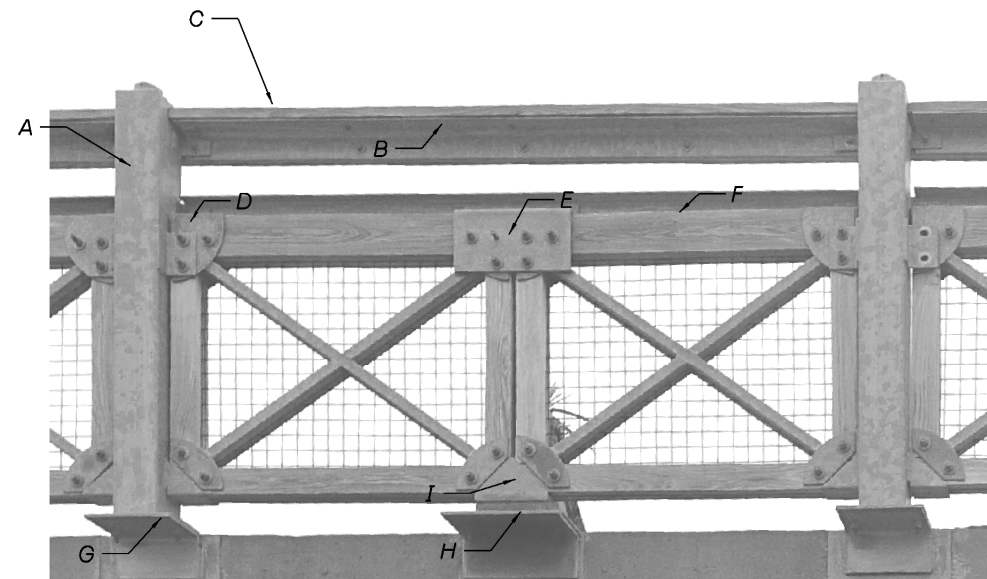


BRIDGE DIVISION.

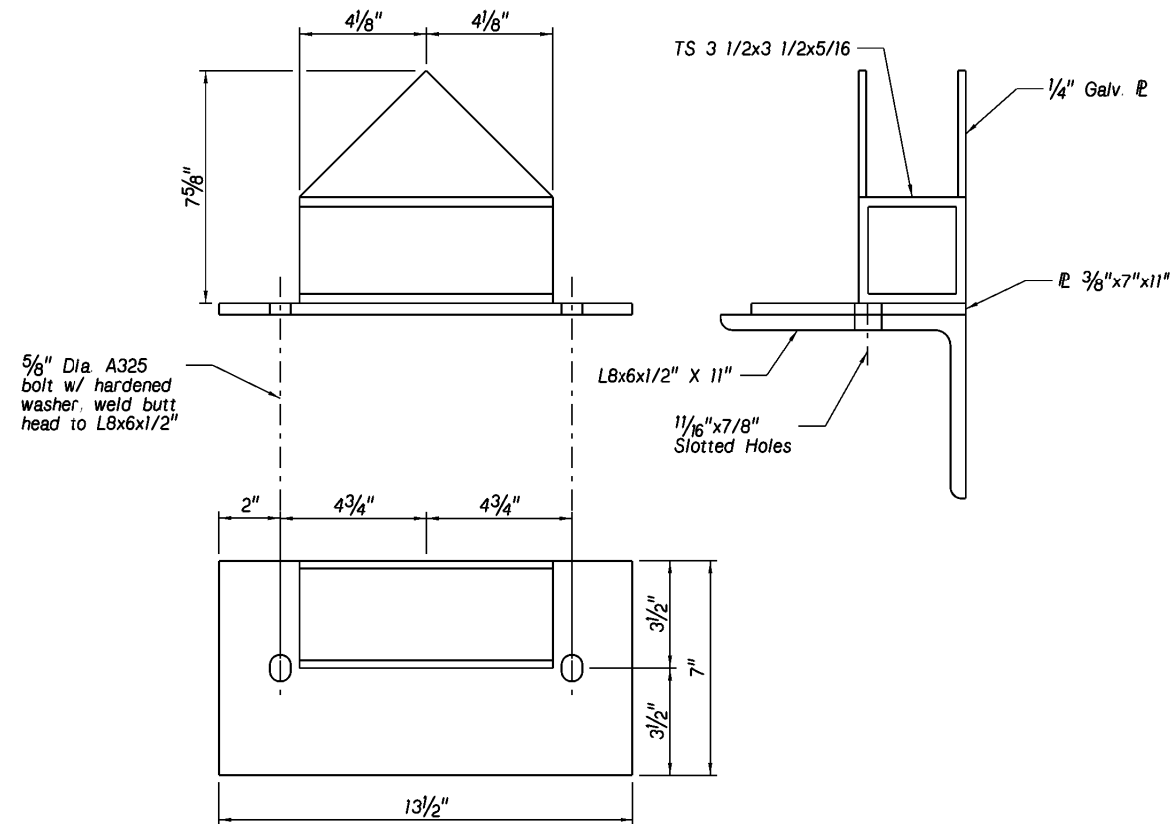
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DATE*****

DGN SPEC*****



- A POST
- B HANDRAIL SUPPORT
- C HANDRAIL
- D UPPER CRADDL
- E MIDDLE CRADDL
- F TIMBER PANEL
- G POST CONNECTION
- H MIDSPAN CONNECTION
- I SADDLE
- J MESH



RAIL NOTES

REINSTALL RAIL SECTIONS
60 Rail Sections, defined by the above detail, have been removed and stored locally at a storage site. The bid item "REINSTALL RAIL SECTIONS" includes all labor, materials and equipment to reinstall all rail sections with the rail connectors (G) and (H)

Epoxy required for reinstalling and/or replacing the connector bolts into the concrete deck shall conform to established list of Nebraska Department of Transportation (NDOT) Approved Products. This approved products list may be found at www.dot.nebraska.gov/business-center/materials/approved-products/

REPAIR RAIL SECTIONS
19 Rail Sections will require repair work to the timber panels (F) and wire mesh panels (J). In addition, 10 - 8 ft runs of top hand rail (C) shall be replaced. The bid item "REPAIR RAIL SECTIONS" includes all labor, materials, and equipment to perform repair work to rail sections.

Mesh used in the rail sections shall be 36"-9 gauge PVC coated welded wire mesh fence.

Timber elements that are replaced shall be CCA treated wood. All wood fasteners shall be counter sunk steel screws that are zinc coated.

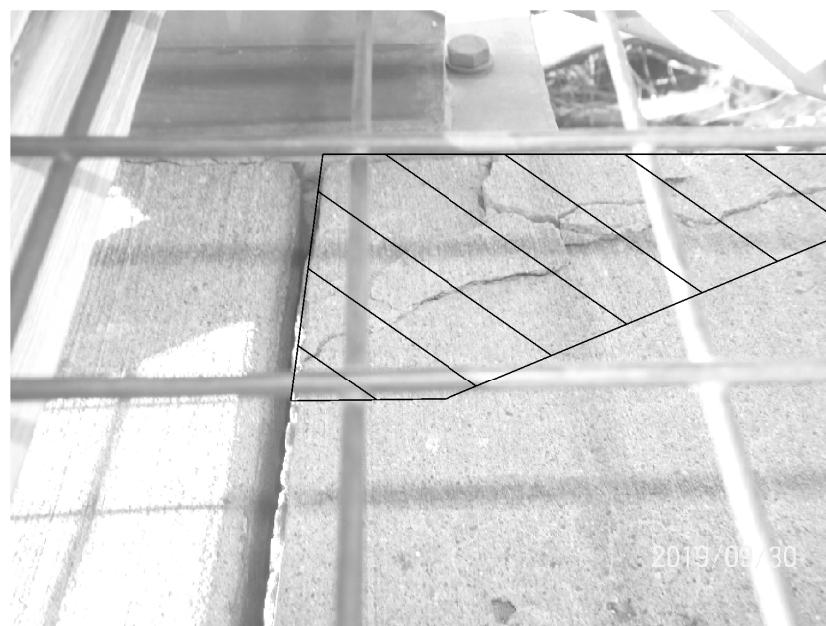
REPAIR RAIL CONNECTORS
The connection of the rail sections to the concrete deck are made with the Post Connector (G) and the Midspan Connector (H). 50 connectors will require repair work prior to installation of the rail sections. 15 of the 50 connectors sustained bent metal of varying degrees. It is anticipated all connectors can be bent back into proper alignment with cold bending or heated bending as needed. All connectors have bolts or threaded rod that has been loosened or displaced from the concrete deck. The contractor shall remove, replace and epoxy grout new 1/2" x 6" H.S. bolts or threaded rod into the concrete deck at these locations. The bid item "REPAIR RAIL CONNECTORS" shall include all labor, materials, and equipment to perform the corrective repair work to the connectors and the connector bolts. Any bolts or rod material used in the repair process shall be galvanized in accordance with ASTM A153.

FABRICATE NEW CONNECTORS
There are 4 Midspan Connectors (H) that are missing and will need to be fabricated and installed. The bid item "FABRICATE NEW CONNECTORS" includes all labor, materials, and equipment to fabricate the connectors in accordance with plan details and install the connectors into the concrete deck. New connectors shall be made from steel meeting the requirements of ASTM A709, Grade 50 and galvanized in accordance with ASTM A123.

BRIDGE DIVISION.



TYPICAL JOINT SPALL



CORNER SPALL



EDGE SPALL

GENERAL NOTES:

Concrete repairs shall be made with approved concrete epoxy selected from the NDOT Approved Products List for structural concrete repairs.

Unsound and loose concrete shall be removed. It is assumed all concrete repairs will be partial depth with 1" to 2" depth as typical. All removed concrete shall be removed from site.

Neat lines shall be scored 3/4" deep around spalled areas. The repair area shall be square in nature and not include any acute angles. Edges of repair shall be a minimum of 6" in length.

Areas to be repaired:

- Joint @ Pier 5
- Joint @ Pier 7
- Joint @ Pier 9
- Joint @ Pier 12
- Joint @ Pier 18
- Joint @ Pier 23
- Edge Spalls (6 locations along rail damaged area)

Care shall be exercised by the contractor so that equipment does not cause additional damage to concrete. No track type equipment shall be allowed on the deck.

COMPUTER***

DATE*****

DGN SPEC*****

SHEET NO.

PROJECT NUMBER

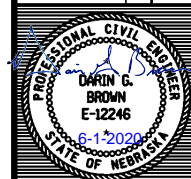
LIED BRIDGE



PAPIO-MISSOURI RIVER NATURAL RESOURCES DISTRICT (PAPIO NRD)

CONCRETE REPAIR DETAILS

DESIGNED BY DGB DETAILED BY CJF CHECKED BY JTP DATE MAY 2020
LIED BRIDGE SUPERSTRUCTURE REPAIR



SCHEMMER